

INTRODUCING

THE

TOP PERFORMANCE

TWIN CAMSHAFT

M G SERIES

**M G A**

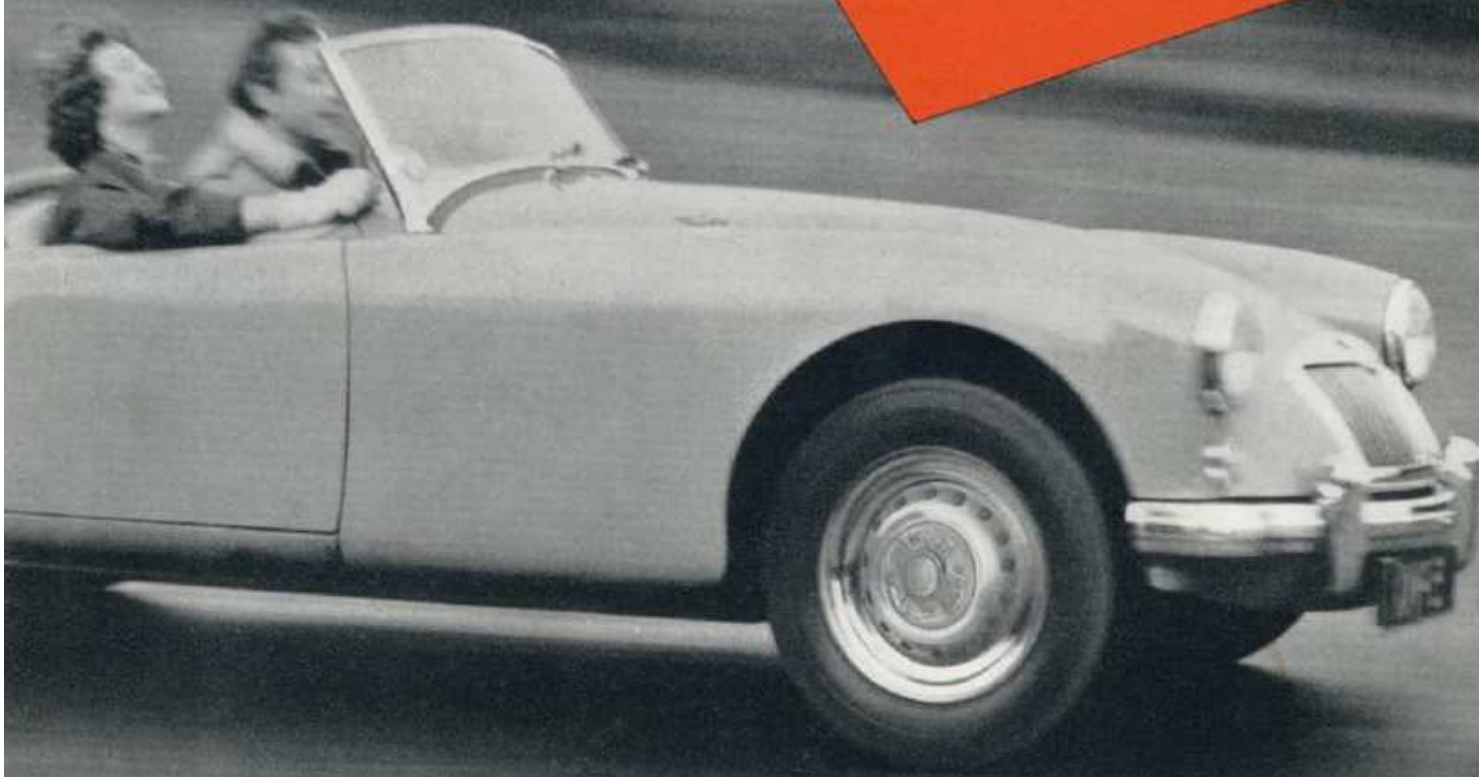
*Safety*



*fast!*

**NOW... A SPECIAL TOP PERFORMANCE**

**MGA**

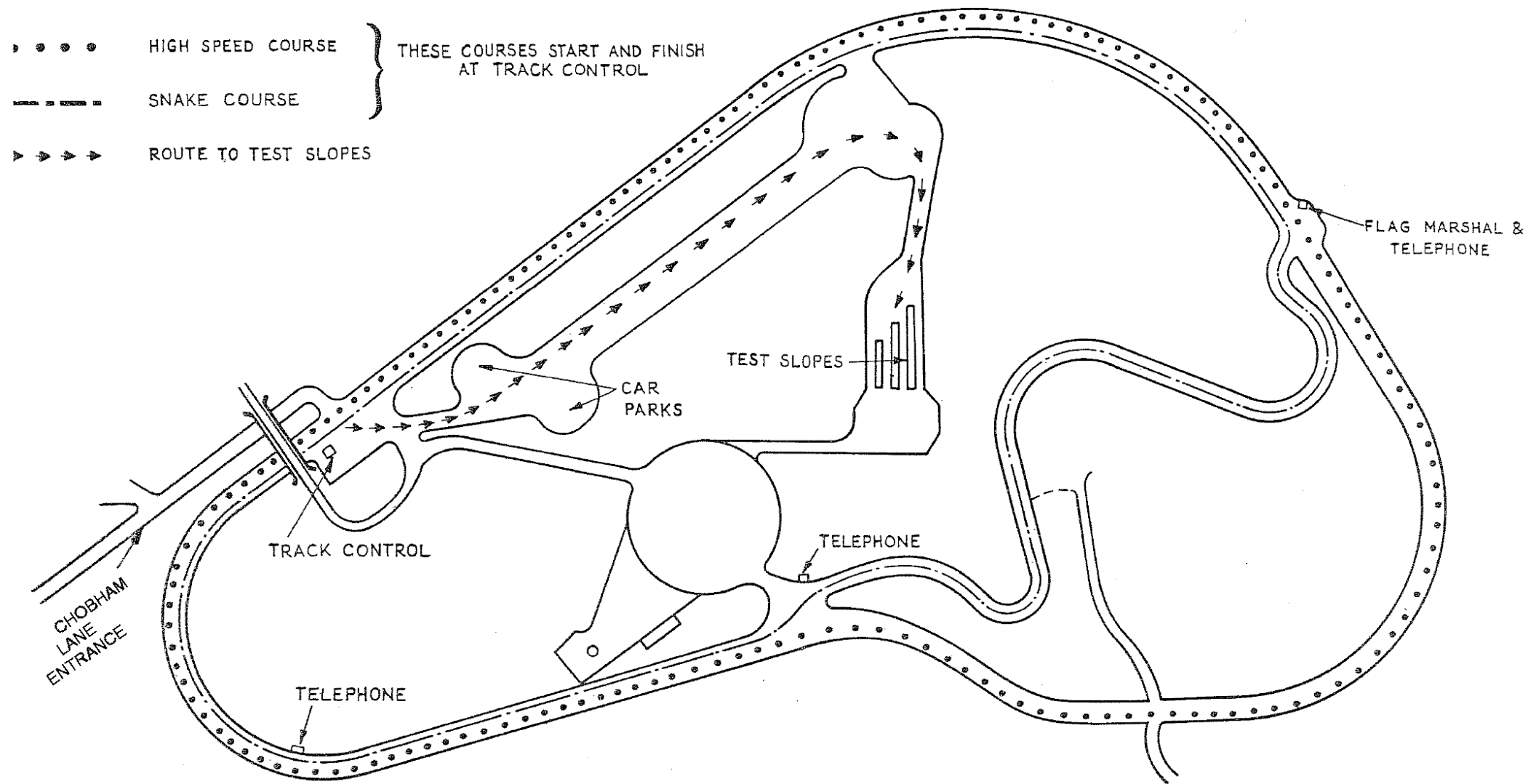


**DISC BRAKES**

**TWIN OVERHEAD CAMSHAFTS**

# THE NUFFIELD ORGANIZATION DEMONSTRATION DAY

## Plan of F. V. R. D. E. Test Courses



# DEMONSTRATION DAY

MORRIS MOTORS BAND BY KIND PERMISSION OF THE VISCOUNT NUFFIELD

The following programme has been set down to allow each guest the chance to test an 'MGA' Twin Cam over three laps of the High-speed Course.

In order that each guest may have this chance, please do not exceed three laps.

You have been assigned to a party distinguishable by the coloured button-hole you are wearing. Please stay with your party.

- 11.00 a.m. Welcome and short briefing.  
Morning coffee available in marquee.
- 11.15 a.m. Start of testing 'MGA' Twin Cams by party wearing RED button-holes—cars stationed at Track Control (3 laps only)—please report to the Marshal.  
Start of testing cars on Snake Course by party wearing YELLOW button-holes—cars stationed at Track Control (3 laps only)—please report to the Marshal.  
Start of testing cars on Test Slopes by party wearing WHITE button-holes—cars stationed at site, transport by Morris J.2 Minibus (6 climbs and descents only)—please report to the Marshal.
- Noon Buffet luncheon available in marquee at Track Control.
- 1.00 p.m. **CHANGE-OVER:**  
Party wearing YELLOW button-holes—take over 'MGA' Twin Cams (3 laps only)—please report to the Marshal.  
Party wearing WHITE button-holes—take over cars on Snake Course (3 laps only)—please report to the Marshal.  
Party wearing RED button-holes—take over cars on Test Slopes (6 climbs and descents only)—please report to the Marshal.
- 2.45 p.m. **CHANGE-OVER:**  
Party wearing WHITE button-holes—take over 'MGA' Twin-Cams (3 laps only)—please report to the Marshal.  
Party wearing RED button-holes—take over cars on Snake Course (3 laps only)—please report to the Marshal.  
Party wearing YELLOW button-holes—take over cars on Test Slopes (6 climbs and descents only)—please report to the Marshal.

## VERY IMPORTANT NOTE:

It is most important, for all-round safety, that the following be observed:

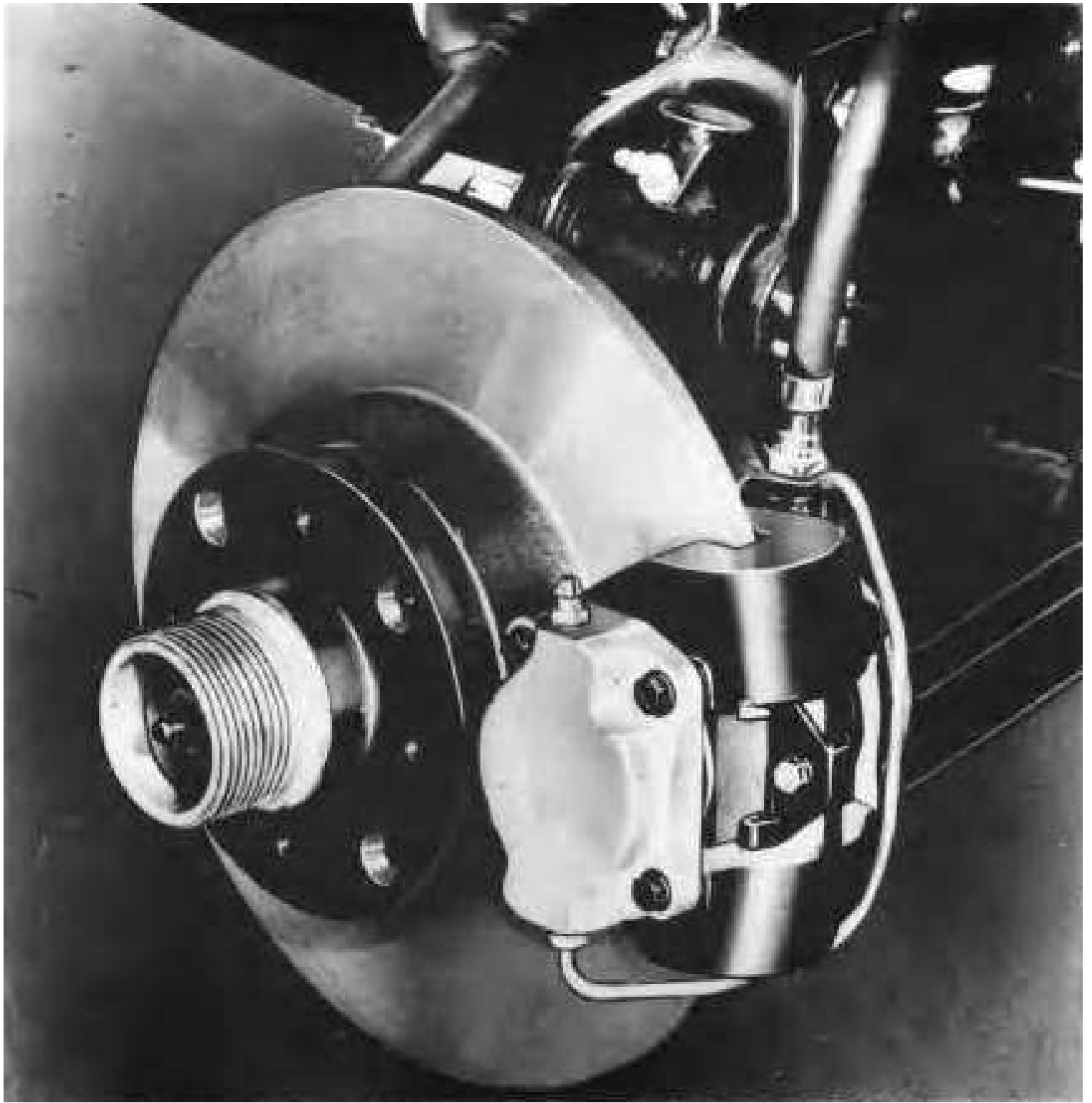
- (1) Travel on High-speed Course to be in anti-clockwise direction, i.e. turn LEFT on leaving Track Control.
  - (2) Obey all light signals
  - (3) Obey all flag signals
- } Red means 'STOP'. Green means 'GO'.
- (4) All drivers of the 'MGA' Twin Cams must keep on the RIGHT-hand side of the High-speed Course.
  - (5) All drivers on the Snake Course must keep on the LEFT-hand side of the High-speed Course.

Since the High-speed Course and Snake Course overlap in part, Notes (4) and (5) have been designed to allow normal use of the Highway Code, i.e. faster vehicles to overtake on the RIGHT.

All drivers of the 'MGA' Twin Cams must, therefore, consider themselves as being in the faster cars and stay on the RIGHT all the time! But please *watch your rear quarters when re-entering Track Control* and use your electric indicators well in advance.

Drivers on the Snake Course must reduce speed in the main straight on the High-speed Course.

**P.S.**—If you do not wish to drive personally and cannot arrange a journey with a fellow journalist, please see the Marshal at the respective car depot, who will arrange for a driver for you.



## ‘Twin Cam’ ‘MGA’

Dunlop disc brakes fitted to all four wheels give the 'Twin Cam' the braking efficiency to match its performance.

Dunlop Scheibenbremsen an allen vier Rädern sichern dem "Zwei-Nocker" Bremseigenschaften, die seinen hohen Fahrleistungen angepasst sind.

Les freins à disques Dunlop sur quatre roues garantissent à la <<Twin Cam>> un freinage adéquat et conforme aux performances.

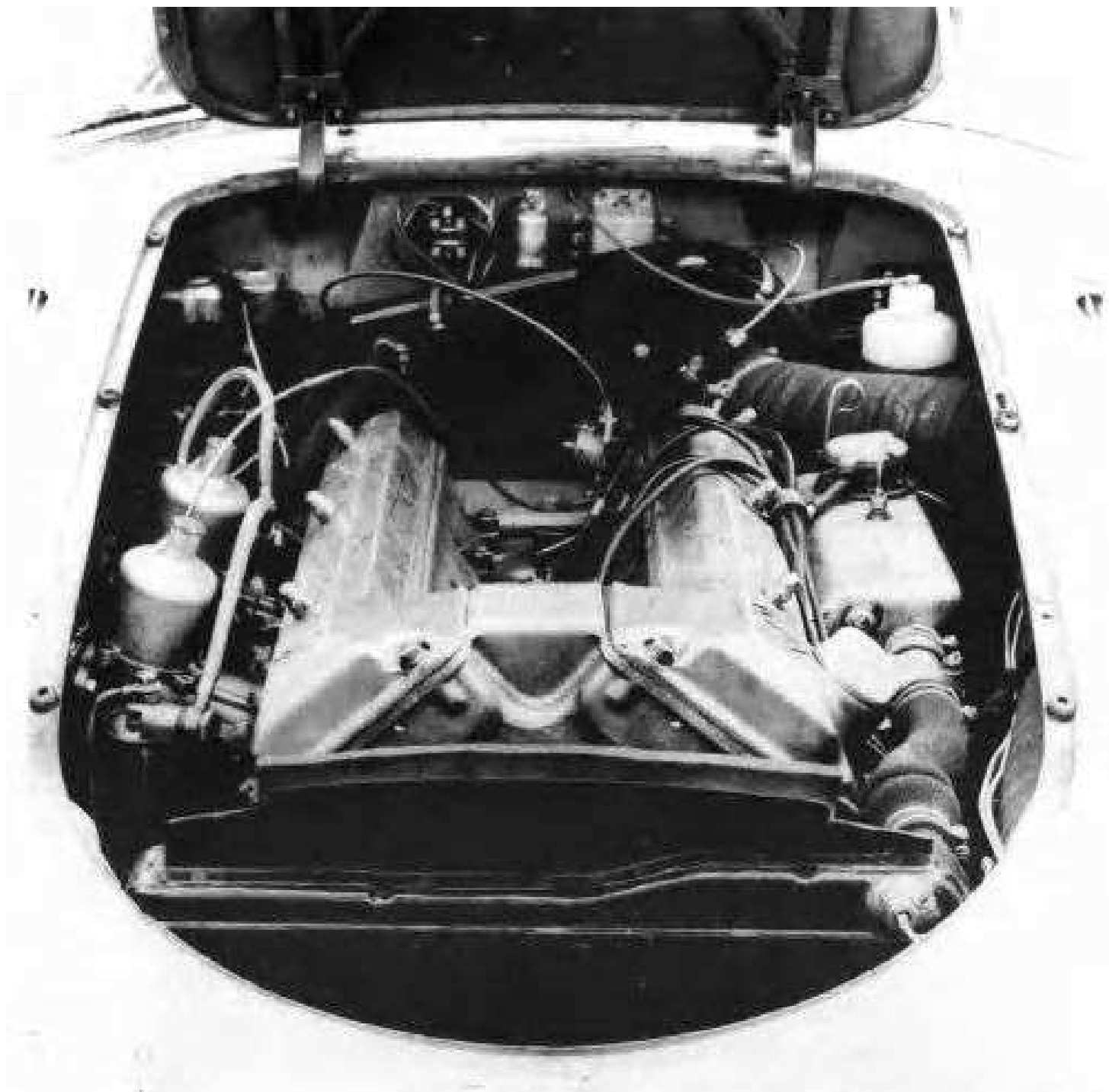


## ‘Twin Cam’ ‘MGA’

The 'Twin Cam' goes well with its o.h.c. engine and 120 m.p.h. speed, stops well with its disc brakes on all four wheels, and looks well with its aerodynamic body and knock-on disc wheels.

Der "Zwei-Nocker" läuft gut dank seines Motors mit zwei obenliegenden Nockenwellen und mit einer Spitze von über 190 Std./km.; er stoppt gut dank den Scheibenbremsen an allen vier Rädern; und er sieht gut aus mit seiner aerodynamischen Karosserie und den Scheibenrädern mit Zentralverschluss.

La «Twin Cam» offre de hautes performances et une vitesse maximum de 190 km. à l'heure grace à son moteur à double arbre à cames en tête; les freins à disques sur quatres roues garantissent toute sécurité; la carrosserie aérodynamique et les roues à disques avec fixation centrale présentent bien.

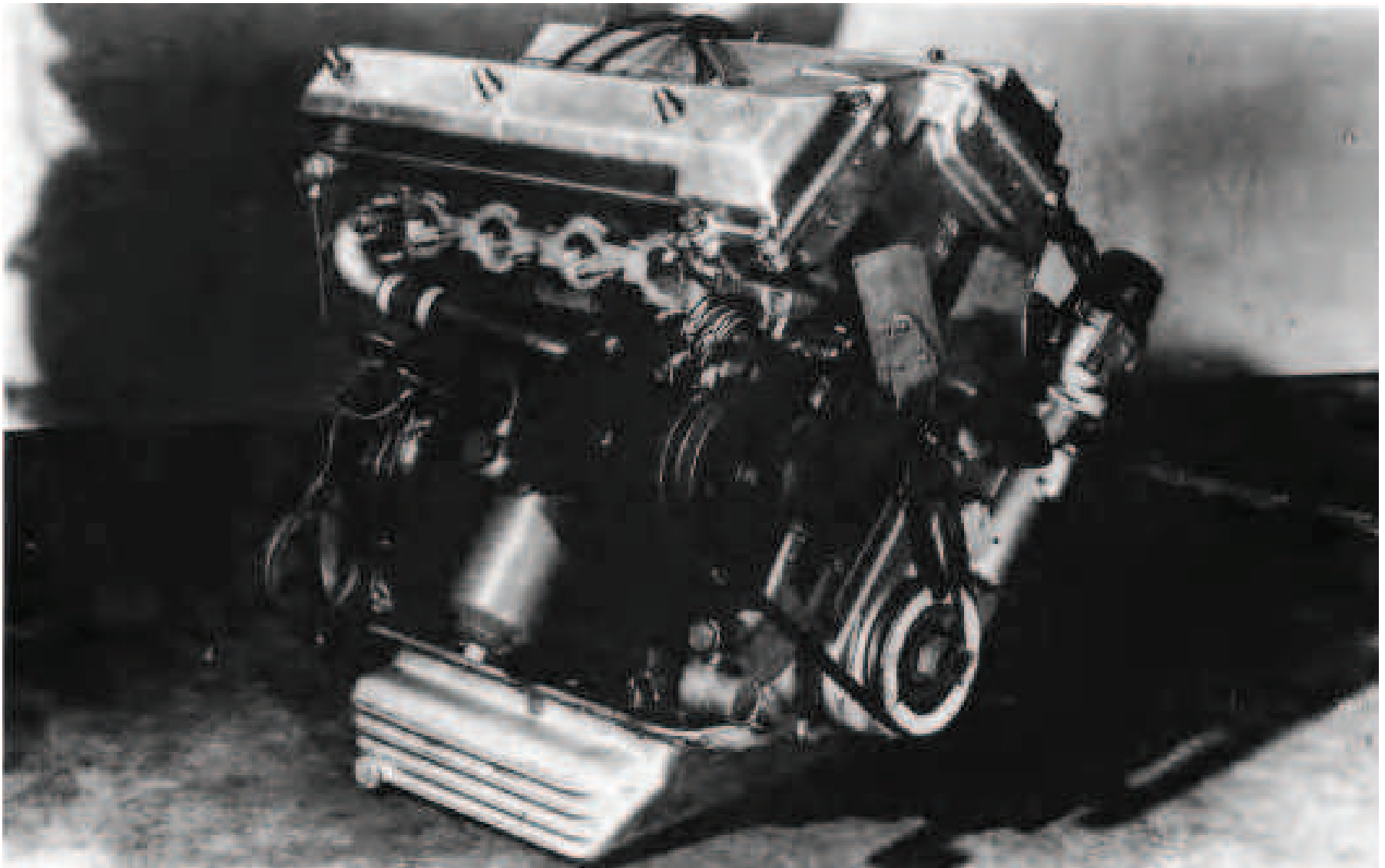


## ‘Twin Cam’ ‘MGA’

The polished aluminum camshaft covers distinguish the engine of the 'Twin Cam'. At left are the twin H6 S.U. carburetors and at right is the radiator header tank..

Zwei polierte Aluminium-Zylinderkopfdeckel kennzeichnen den "Zwei-Nocken"-Motor. Links die zwei S.U. H6 Vergaser und rechts der obere Wasserkasten des Kühlsystems.

Le moteur <<Twin Cam>> se distingue par ses deux couvercles en aluminium poli au-dessus des arbres à cames. A gauche se trouvent les deux carburateurs S.U. H6 et à droite le réservoir supérieur du système de refroidissement.



## ‘Twin Cam’ ‘MGA’

The intake side of the engine of the 'Twin Cam' reveals the ports which lie behind the carburetters. The sump is of cast aluminium and has fins to aid cooling..

Die Ansaugseite des "Zwei-Nocker's" zeigt die hinter den Vergasern liegenden Einlasskanäle. Die Leichtmetall-Oelwanne ist zwecks Kühlung des Motorenöls gerippt.

Le côté d'admission de la «Twin Cam» montre les tubulures d'admission se trouvant derrière les deux les carburateurs. Le carter en aluminium est nervuré dans l'intèret du refroidissement de l'huile..